



**IFB NO 23004-FY23-45
LEESBURG EXECUTIVE AIRPORT
NORTH APRON PAVING
ADDENDUM NO. 2**

March 23, 2023

ITEM NO. 1: PRE-BID MEETING AGENDA, PRESENTATION, AND SIGN-IN SHEET

Town staff held a pre-bid meeting which included several documents as follows:

- Agenda – see Attachment A
- Presentation – see Attachment B
- Sign-in sheet – see Attachment C

ITEM NO. 2: QUESTIONS AND RESPONSES DURING PRE-BID MEETING

Interested bidders shall be mindful of the following responses to questions asked:

1. Question: Is there an engineer's estimate cost for this project?
RESPONSE: The FY2023 Capital Improvements Program includes a total budget of \$936,000 for the project, inclusive of project management, design and construction.
2. Question: Any opportunity for time extension?
RESPONSE: Delays resulting from the Contractor's actions, or actions of others within the Contractors' control or influence will not be considered grounds to extend the contract date. In the event of an abnormal weather delay for which the Contractor is seeking a time extension, the Town will evaluate the delay impact on the project completion.
3. Question: Will drainage work start at the low point?
RESPONSE: Yes.
4. Question: Is a DEQ permit required?
RESPONSE: No.
5. Question: Will this project be subject to asphalt escalation?
RESPONSE: No.
6. Question: Is there a "hold" date? A critical date?
RESPONSE: The Phase 1 work is on the critical path and the total number of days to Substantial Completion is 60 calendar days from Notice to Proceed.

Bidders must take due notice and be governed accordingly. This addendum must be acknowledged as indicated in the Invitation for Bid or your bid may not be considered.

For the Town of Leesburg,

Cindy Steyer, CPPB, VCO

Senior Contract Specialist

Town of Leesburg, Virginia

Email: CapitalBidQuestions@leesburgva.gov

Bid Board: <http://www.leesburgva.gov/bidboard>

END OF ADDENDUM NO. 2



**PRE-BID MEETING AGENDA
IFB NO. 23004-FY23-45
LEESBURG EXECUTIVE AIRPORT
NORTH APRON PAVING**

Introduction

- Town Representatives
 - Dave Christianson, Deputy Procurement Officer
 - Karin Franklin, Senior Engineer
 - Walid Quttaineh, Project Manager for Construction & Engineering
 - Scott Coffman, Airport Director
 - Steve Peterson, Talbert & Bright
- Interested Bidders

Pertinent Bid Information (*Procurement/David*)

- All information regarding this project can be found on the Town's Bid Board: <http://www.leesburgva.gov/bidboard>. To receive updates, such as addenda, please register to receive updates on the Town's Bid Board
- Up until contract award, **all** questions must be submitted in writing to: capitalbidquestions@leesburgva.gov
- Question deadline is 5:00 p.m. ET on Tuesday, March 28, 2023
- Bids are due on Thursday, April 13, 2023 @ 3:00 p.m. ET. (unless modified via addendum)
- Bid Submission:
 - Bids shall be submitted electronically via the Commonwealth's eProcurement website, eVA: www.eva.virginia.gov
 - No late bids will be accepted (including partially uploaded bids)
- Required Bid Response Forms:
 - Pages 9-21 of IFB
- Bid Bond:
 - An original copy of the bid bond is not required to be submitted to the Town; however, the Town reserves the right to require any bidder to submit a wet signature, raised seal bid bond at the Town's request.
 - When the bid security is in the form of a cashier's check, the original cashier's check shall be delivered to the Town within two (2) business days after the bids are due to the Town.
- Bid Opening:
 - At the bid due date/time, a public bid opening will be held via Microsoft Teams
- Insurance
 - General Liability \$3M
 - Auto: \$3M
 - Workers' Compensation per State Law

Scope of Work & Specifications (*Capital Projects/Talbert & Bright*)

- The purpose of this IFB is to establish a contract with a qualified contractor to construction services, including, but not limited to:
 - Pavement rehabilitation
 - Full Depth reclamation – 16"
 - Pavement milling



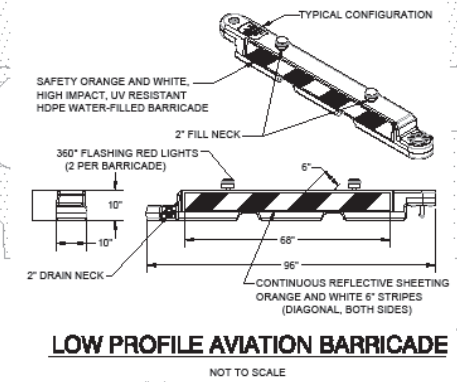
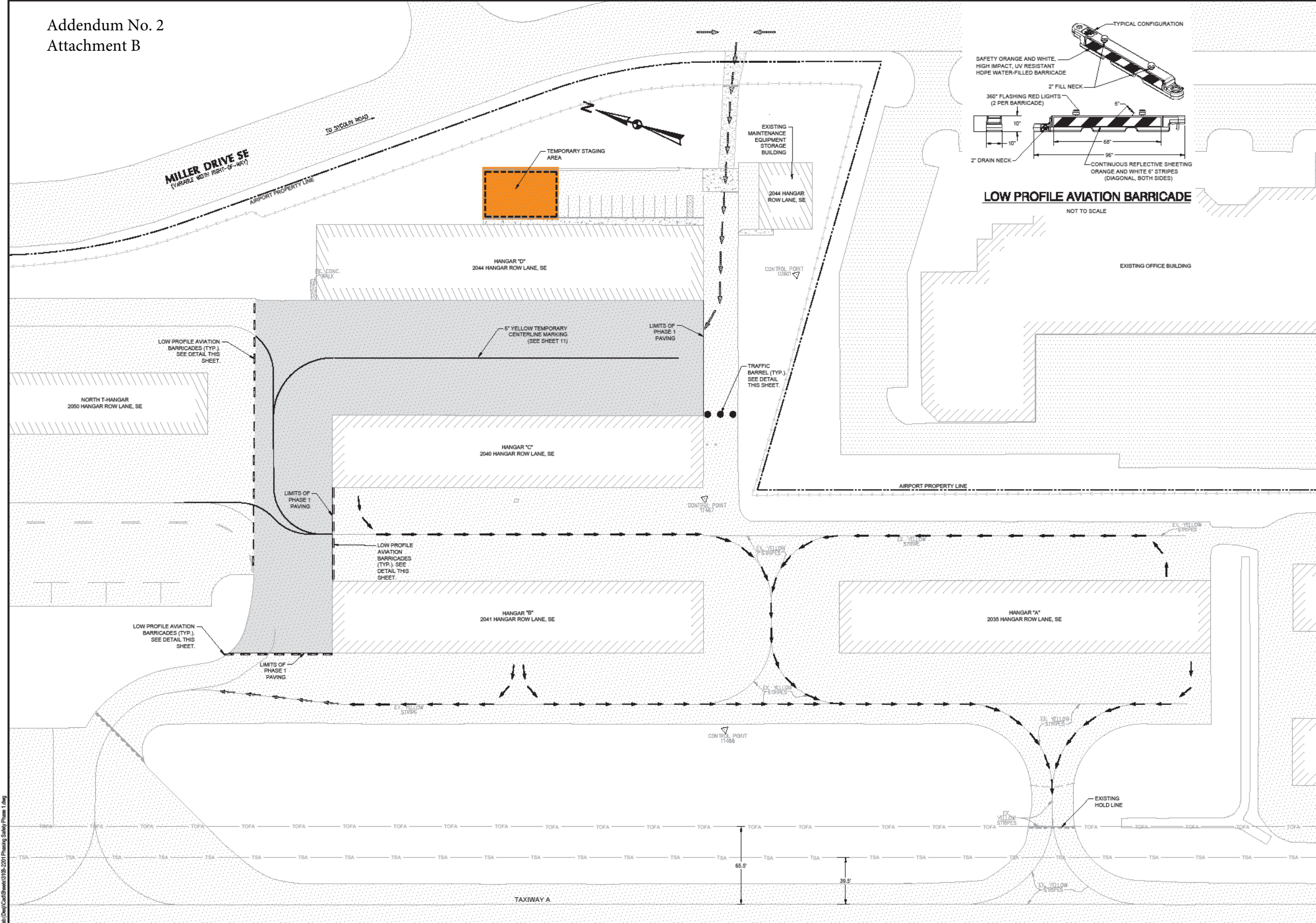
- Removal of excess material
- Miscellaneous demolition
- Placement of double chip seal
- Asphalt placement (VDOT state mix)
- Maintenance of traffic (on airport)
- Underdrain installation
- Pavement marking
- Contract Time:
 - Notice to Proceed: Anticipated for summer 2023.
 - Substantial Completion: 60 calendar days from Notice to Proceed
 - Final Completion: 30 calendar days from Substantial Completion
- Liquidated Damages: \$2,000.00 per day
- Project funding – combination of DOAV and Town of Leesburg, VA
- Earliest start month – July 2023
- General overview of Table of contents (Contractor is encouraged to read all specifications)
 - Instructions to Bidders & Scope of Work: Pages 22-30 of IFB
 - Advertisement
 - Contract Pay Items of Work: Please refer to the Bid Form and Contract Items of Work sections of the IFB
 - Sample agreement including bonds
 - General Conditions: GC1-GC85
 - Project General Provisions: Sections 10, 20 and 40
 - Project Special Provisions: Section PSP
 - Haul road
 - Safety
 - Utility protection
 - Dust control
 - Dewatering
 - Shop drawings
 - Construction Layout
 - Quality control program
 - Technical Specifications: Sections C-102 – VA-SP315
 - Letter Report of Geotechnical Services – Attachment A
 - Bid Plans: Sheets 1-16
- Construction Plan Overview

Questions/Answers

Site Visit

- To immediately follow pre-bid meeting.

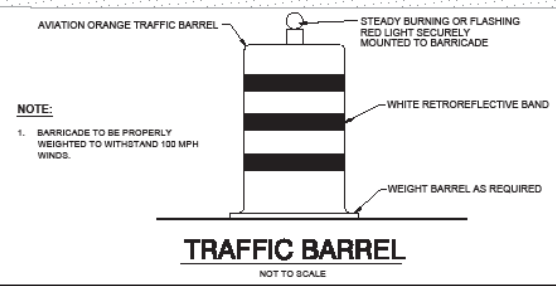
Addendum No. 2
Attachment B



LEGEND		
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT		
TAXIWAY OBJECT FREE AREA		TOFA
TAXIWAY SAFETY AREA		TSA
LOW PROFILE AVIATION BARRICADE		
PHASE 1 WORK AREA		
AIRCRAFT TAXI ROUTE		
TIE-DOWN		
BUILDING		
HAUL ROUTE		
PROPERTY LINE		
CONTROL POINT		
FENCE		
GRAVEL		

- AVIATION BARRICADE NOTES:**
- THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 - LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE, LIGHTS SHALL BE RED.
 - EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 - CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 - BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP).

- PHASING NOTES:**
- PHASE 1**
- ESTABLISH STAGING AREA.
 - INSTALL LOW PROFILE BARRICADES TO THE LOCATIONS DEPICTED FOR PHASE 1 WORK AREA.
 - PHASE 1 WORK AREA WILL IMPACT THE FOLLOWING:
 - HANGAR "D" EAST SIDE OF HANGAR "C" NORTH HANGAR DEVELOPMENT AREA (26-UNIT T-HANGAR)
 THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
 - INSTALL TRAFFIC DRUMS.
 - PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 1 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 - PERFORM FULL DEPTH RECLAMATION, TRENCH DRAIN CONCRETE IMPROVEMENTS AND ASPHALT PLACEMENT.
 - PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 1 AREA ONLY.
 - WORK IN PHASE 1 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO CLOSING OF PHASE 3 WORK AREA.
 - PHASE 1 CONTRACT TIME - 20 CALENDAR DAYS.



SHEET INDEX

1" = 40'

Scale: 1" = 40'

Drawn: REK

Checked: STP

Project No.: 3108-2201

Sheet No.: **4**

of 16

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1010 KRAUSE ROAD, SUITE 100
DREES FERRY, VIRGINIA 22623
PHONE: 804-666-8676 FAX: 804-666-8671

PHASING & SAFETY PLAN - PHASE 1
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

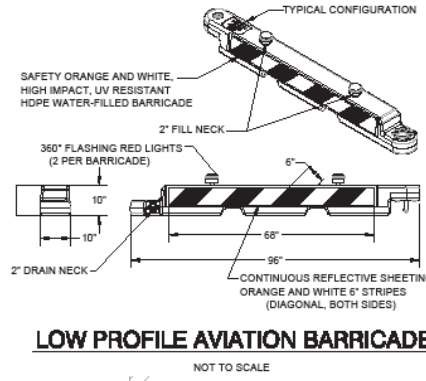
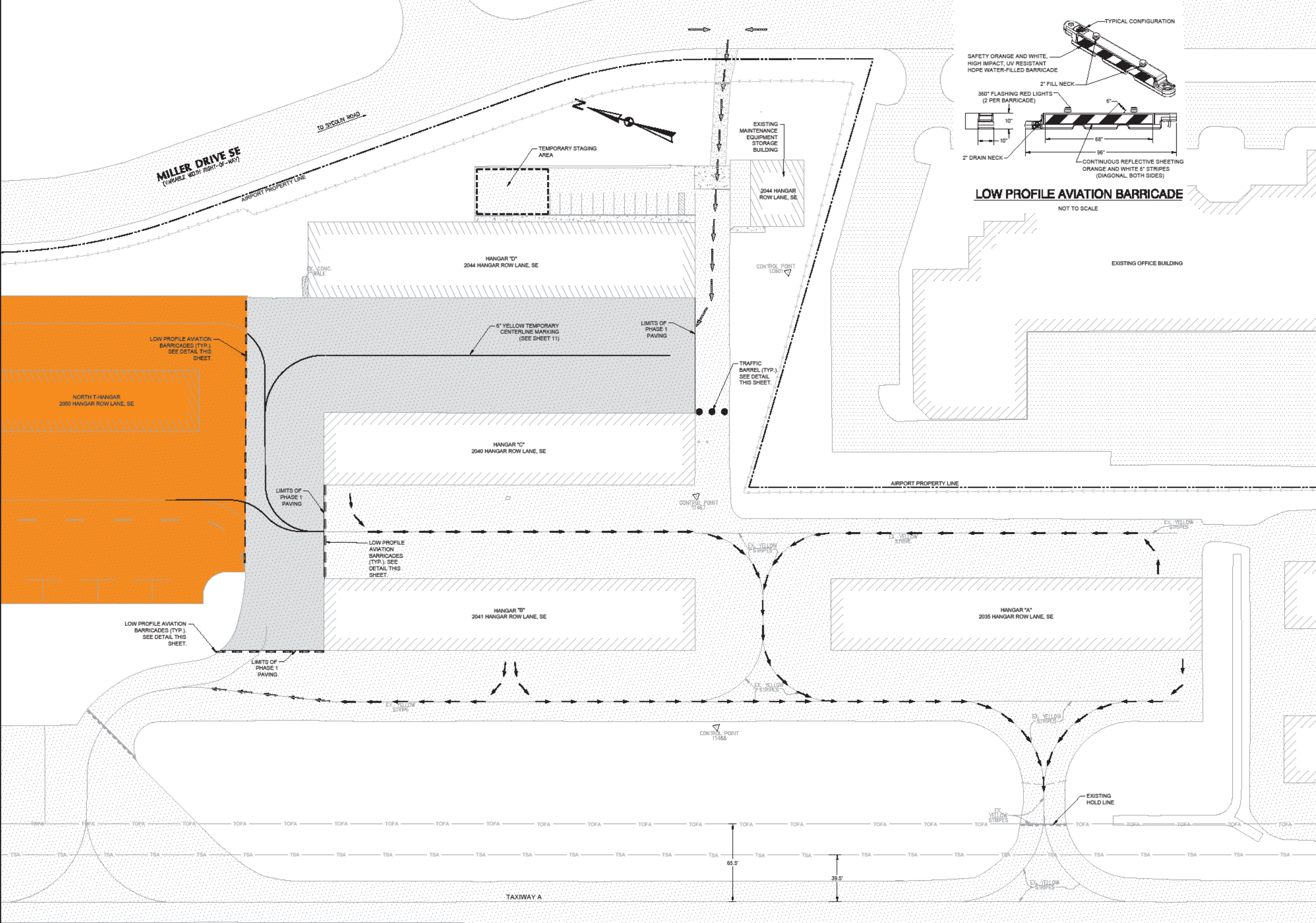
Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	REK
Checked	STP
Project No.	3108-2201
Sheet No.	4
of	16

E:\Projects\3108-2201\3108-2201_Paving_Safety_Phase_1.dwg

S:\projects\1010116-2011 Hangar A, B, C & D Pavement Rehab\Drawings\1010116-2011 Phase 1.dwg

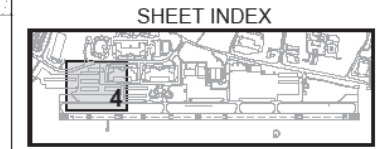
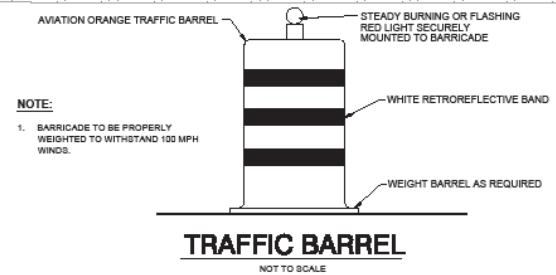


LOW PROFILE AVIATION BARRICADE
NOT TO SCALE

LEGEND		
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT		
TAXIWAY OBJECT FREE AREA		TOFA
TAXIWAY SAFETY AREA		TSA
LOW PROFILE AVIATION BARRICADE		—
PHASE 1 WORK AREA		
AIRCRAFT TAXI ROUTE		→
TIE-DOWN		⊕
BUILDING		
HAUL ROUTE		→
PROPERTY LINE		—
CONTROL POINT		▽
FENCE		—
GRAVEL		

- AVIATION BARRICADE NOTES:**
- THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 - LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE. LIGHTS SHALL BE RED.
 - EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 - CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 - BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP).

- PHASING NOTES:**
- PHASE 1**
- ESTABLISH STAGING AREA.
 - INSTALL LOW PROFILE BARRICADES TO THE LOCATIONS DEPICTED FOR PHASE 1 WORK AREA.
 - PHASE 1 WORK AREA WILL IMPACT THE FOLLOWING:
HANGAR "D"
EAST SIDE OF HANGAR "C"
NORTH HANGAR DEVELOPMENT AREA (26-UNIT T-HANGAR)
THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
 - INSTALL TRAFFIC DRUMS.
 - PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 1 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 - PERFORM FULL DEPTH RECLAMATION, TRENCH DRAIN CONCRETE IMPROVEMENTS AND ASPHALT PLACEMENT.
 - PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 1 AREA ONLY.
 - WORK IN PHASE 1 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO CLOSING OF PHASE 3 WORK AREA.
 - PHASE 1 CONTRACT TIME - 20 CALENDAR DAYS.



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
10105 KILGUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23838
PHONE: 804-666-6876 FAX: 804-666-8871

NO.	DATE	DESCRIPTION	REVISIONS

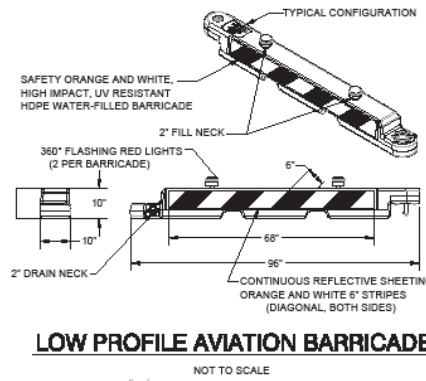
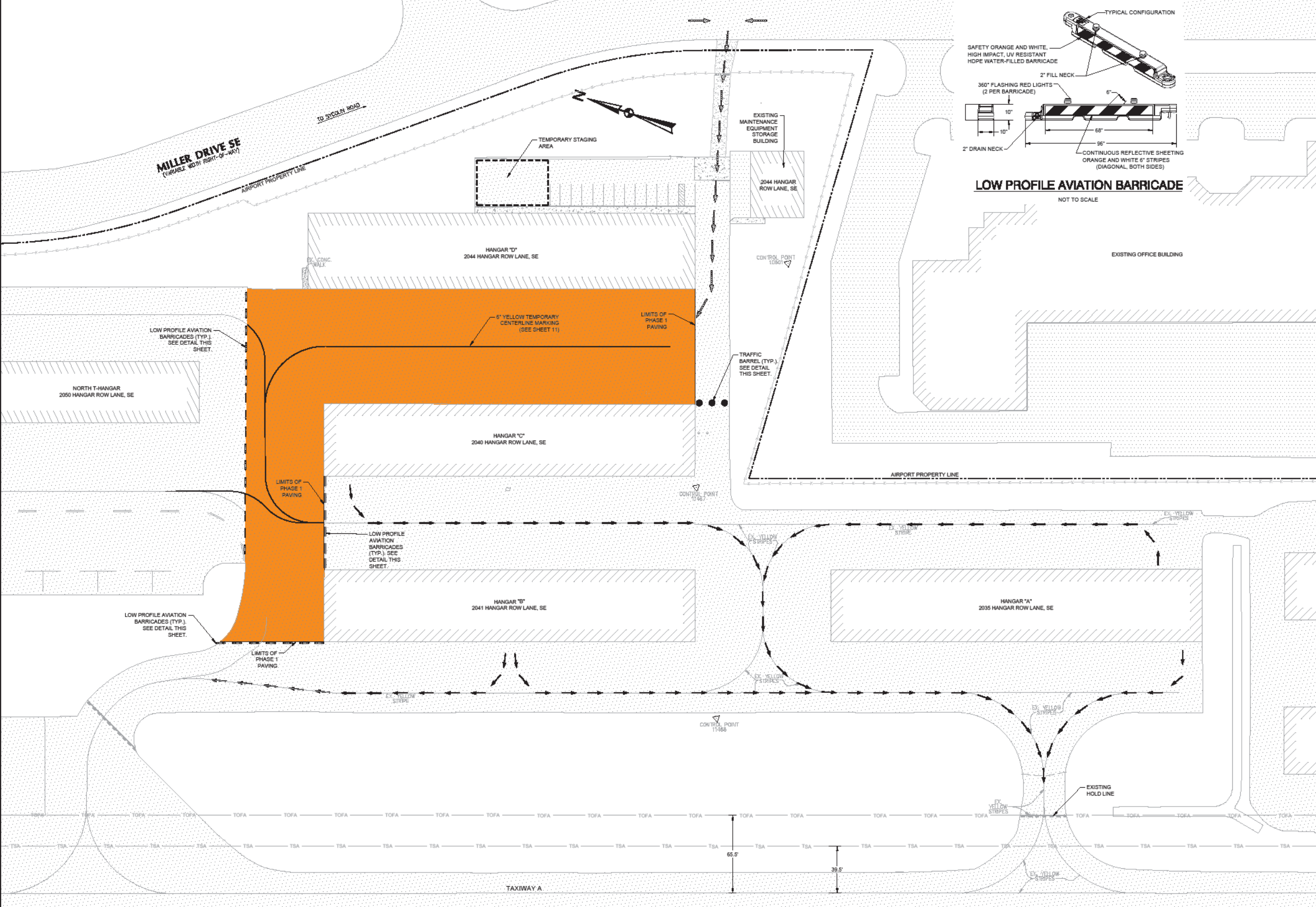
PHASING & SAFETY PLAN - PHASE 1
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date: FEBRUARY 2023
Scale: 1"=40'
Drawn: REX
Checked: STP
Project No.: 3108-2201
Sheet No.: 4
of 16

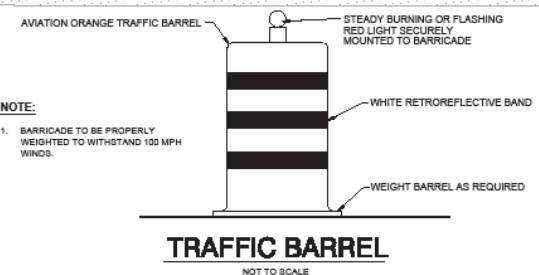
Streetscape/Aviation (1/17/2018-2021) Hangars A, B, C & D Pavement Rehabilitation (Final) (C:\Users\jsh1516\OneDrive\Projects\2018-2021\Phase 1\Phase 1.dwg)



	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT	[Pattern]	[Pattern]
TAXIWAY OBJECT FREE AREA	[Symbol]	[Symbol]
TAXIWAY SAFETY AREA	[Symbol]	[Symbol]
LOW PROFILE AVIATION BARRICADE	[Symbol]	[Symbol]
PHASE 1 WORK AREA	[Symbol]	[Symbol]
AIRCRAFT TAXI ROUTE	[Symbol]	[Symbol]
TIE-DOWN	[Symbol]	[Symbol]
BUILDING	[Symbol]	[Symbol]
HAUL ROUTE	[Symbol]	[Symbol]
PROPERTY LINE	[Symbol]	[Symbol]
CONTROL POINT	[Symbol]	[Symbol]
FENCE	[Symbol]	[Symbol]
GRAVEL	[Symbol]	[Symbol]

- AVIATION BARRICADE NOTES:**
1. THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 2. LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE, LIGHTS SHALL BE RED.
 3. EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 4. CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 5. BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP).

- PHASING NOTES:**
- PHASE 1**
1. ESTABLISH STAGING AREA.
 2. INSTALL LOW PROFILE BARRICADES TO THE LOCATIONS DEPICTED FOR PHASE 1 WORK AREA.
 3. PHASE 1 WORK AREA WILL IMPACT THE FOLLOWING:
 HANGAR "D"
 EAST SIDE OF HANGAR "C"
 NORTH HANGAR DEVELOPMENT AREA (26-UNIT T-HANGAR)
 THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
 4. INSTALL TRAFFIC DRUMS.
 5. PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 1 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 6. PERFORM FULL DEPTH RECLAMATION, TRENCH DRAIN CONCRETE IMPROVEMENTS AND ASPHALT PLACEMENT.
 7. PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 1 AREA ONLY.
 8. WORK IN PHASE 1 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO CLOSING OF PHASE 3 WORK AREA.
 9. PHASE 1 CONTRACT TIME - 20 CALENDAR DAYS.



SHEET INDEX

1" = 40' scale

0 40 80 feet

TALBERT & BRIGHT
 ENGINEERING & PLANNING CONSULTANTS
 10105 KRAUSE ROAD, SUITE 100
 RESTON, VIRGINIA 20190
 PHONE: 804-666-8676 FAX: 804-666-8671

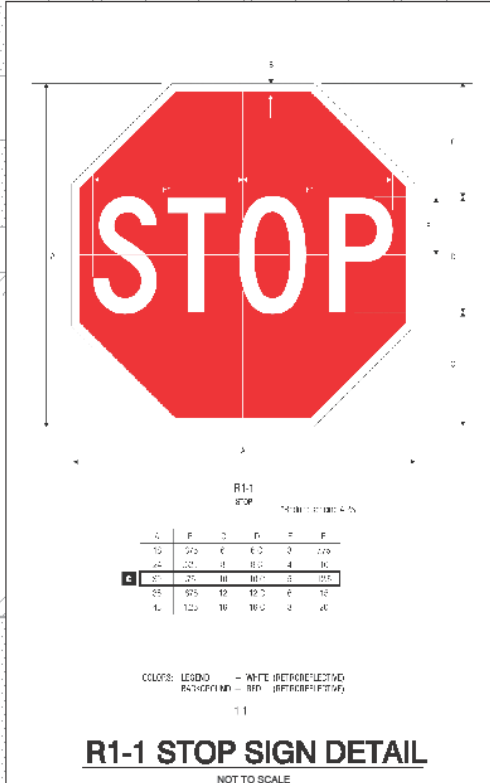
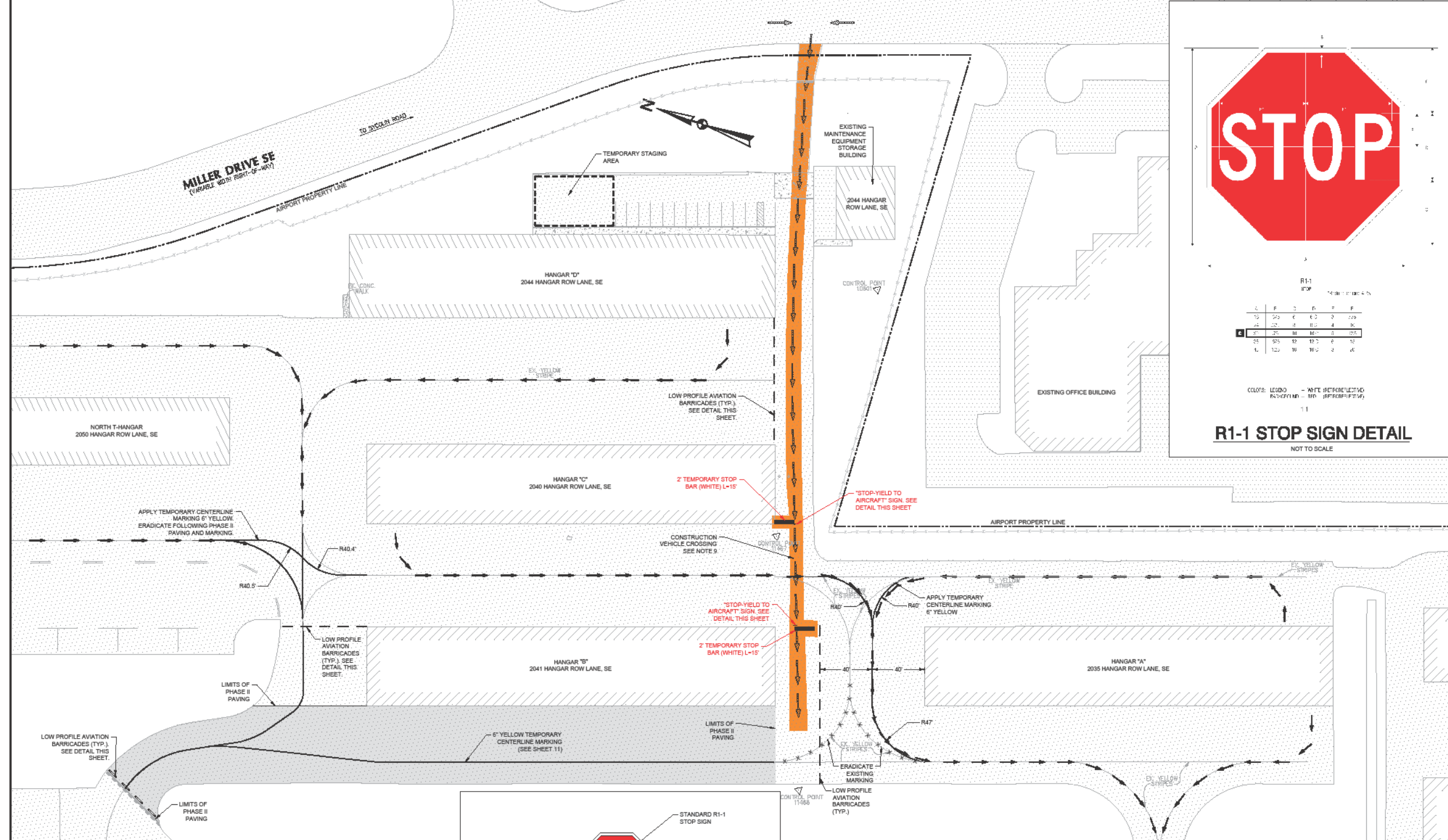
REV	DESCRIPTION	DATE

PHASING & SAFETY PLAN - PHASE 1
 Hangars A, B, C & D Pavement Rehabilitation
 DOAV No. CS0027-45
 Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.

Date: FEBRUARY 2023
 Scale: 1"=40'
 Drawn: REX
 Checked: STP
 Project No.: 3108-2201
 Sheet No.: **4** of 16

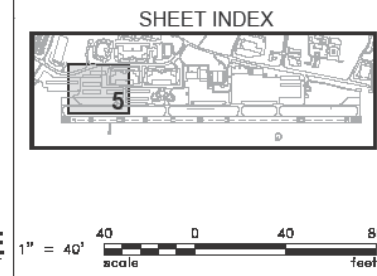
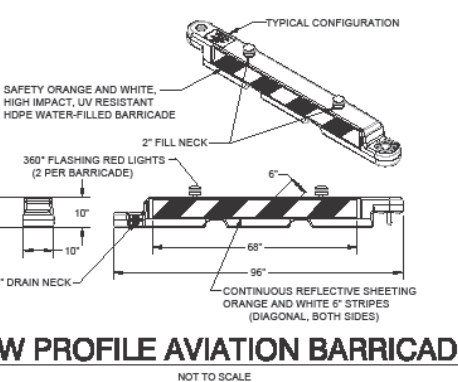
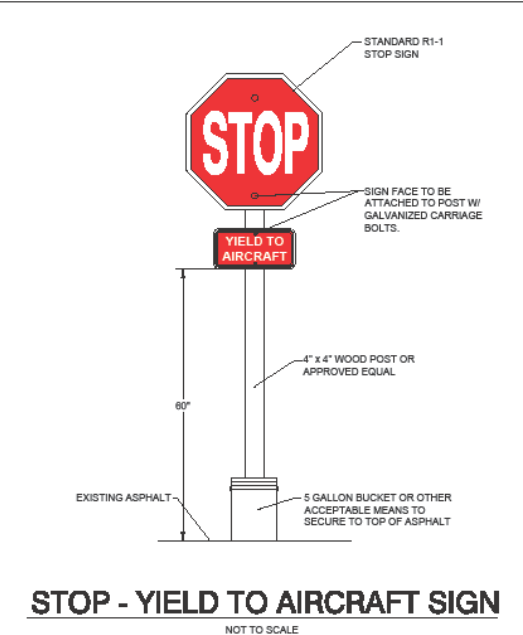
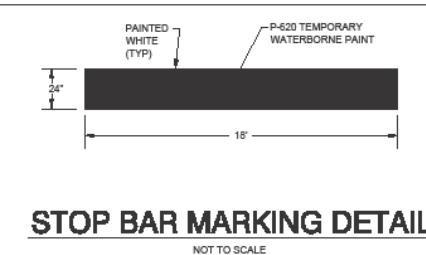
Storobinsky [UTD]108-2201 Hangars A, B, C & D Pavement Rehabilitation (Det) Civil/Sheet/1108-2201 Phasing Safety Phase 2.dwg



	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT	[Pattern]	[Pattern]
TAXIWAY OBJECT FREE AREA	[Pattern]	[Pattern]
TAXIWAY SAFETY AREA	[Pattern]	[Pattern]
LOW PROFILE AVIATION BARRICADE	[Pattern]	[Pattern]
PHASE 1 WORK AREA	[Pattern]	[Pattern]
AIRCRAFT TAXI ROUTE	[Pattern]	[Pattern]
TIE-DOWN	[Pattern]	[Pattern]
BUILDING	[Pattern]	[Pattern]
HAUL ROUTE	[Pattern]	[Pattern]
PROPERTY LINE	[Pattern]	[Pattern]
CONTROL POINT	[Symbol]	[Symbol]
FENCE	[Pattern]	[Pattern]
GRAVEL	[Pattern]	[Pattern]

- AVIATION BARRICADE NOTES:**
1. THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 2. LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE. LIGHTS SHALL BE RED.
 3. EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 4. CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 5. BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP.)

- PHASING NOTES:**
- PHASE 2**
1. PHASE 2 WORK AREA WILL IMPACT THE FOLLOWING:
EAST SIDE OF HANGAR "B"
- THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
2. PLACE LOW PROFILE BARRICADES AS PER PLAN.
 3. ERADICATE THE EXISTING CENTERLINE MARKING AS REQUIRED. PLACE TEMPORARY CENTERLINE MARKING, STOP BARS, AND SIGNS.
 4. PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES WITHIN PHASE 2 LIMITS AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 2 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 5. PERFORM FULL DEPTH RECLAMATION, CONCRETE IMPROVEMENTS AND ASPHALT PLACEMENT.
 6. PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 2 AREA ONLY.
 7. WORK IN PHASE 2 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO CLOSING OF PHASE 3 WORK AREA.
 8. PHASE 2 CONTRACT TIME 10 CALENDAR DAYS.
 9. THE CONTRACTOR IS ADVISED THAT THE EXISTING PAVEMENT BETWEEN HANGAR B & C WILL REMAIN OPERATIONAL AND AIRCRAFT TAXIING WILL CONTINUE DURING PHASE 2 WORK. THE CONTRACTOR SHALL ADVISE ALL PERSONNEL (INCLUDING SUBS) OF THE REQUIREMENT TO STOP AND CONFIRM NO AIRCRAFT ARE TAKING THROUGH THE AREA AT THE TIME OF CROSSING. ALL CONSTRUCTION EQUIPMENT SHALL YIELD TO AIRCRAFT AT ALL TIMES. DURING HAULING OPERATIONS, THE CONTRACTOR SHALL POST A COMPETENT EMPLOYEE AT THE INTERSECTION TO DIRECT CONSTRUCTION TRAFFIC TO ENSURE PLANES ARE NOT TAXIING AT THE TIME OF VEHICLE CROSSING.
- AS PART OF THE CONTRACTOR'S PHASING AND SCHEDULE PREPARATION, SCHEDULING BETWEEN THE CONTRACTOR/OWNER/TENANT WILL BE REQUIRED ON A WEEKLY BASIS TO ENSURE MINIMAL IMPACT TO THE TENANTS AND CONTRACTOR ALIKE. NO MATERIAL, REGARDLESS OF NATURE, SHALL BE STOCKPILED ADJACENT TO THE TAXILANE OBJECT FREE AREA (TOFA). EXISTING ASPHALT SHALL BE PROTECTED AT ALL TIMES.



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1005 KRAUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23832
PHONE: 804-686-6676 FAX: 804-686-6671

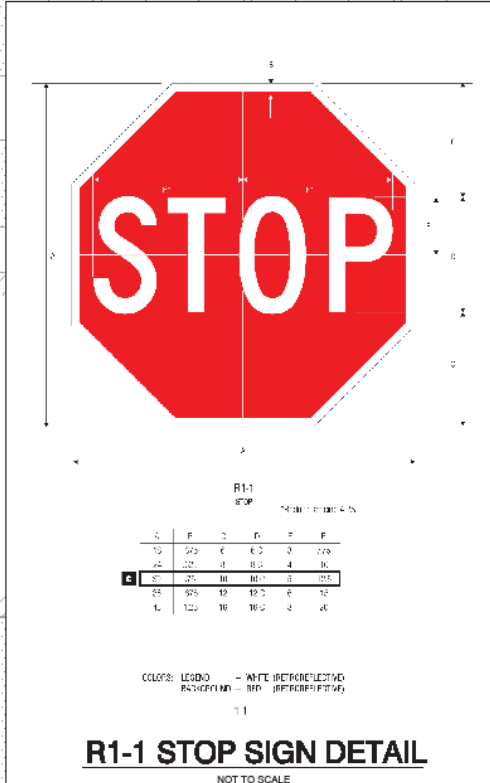
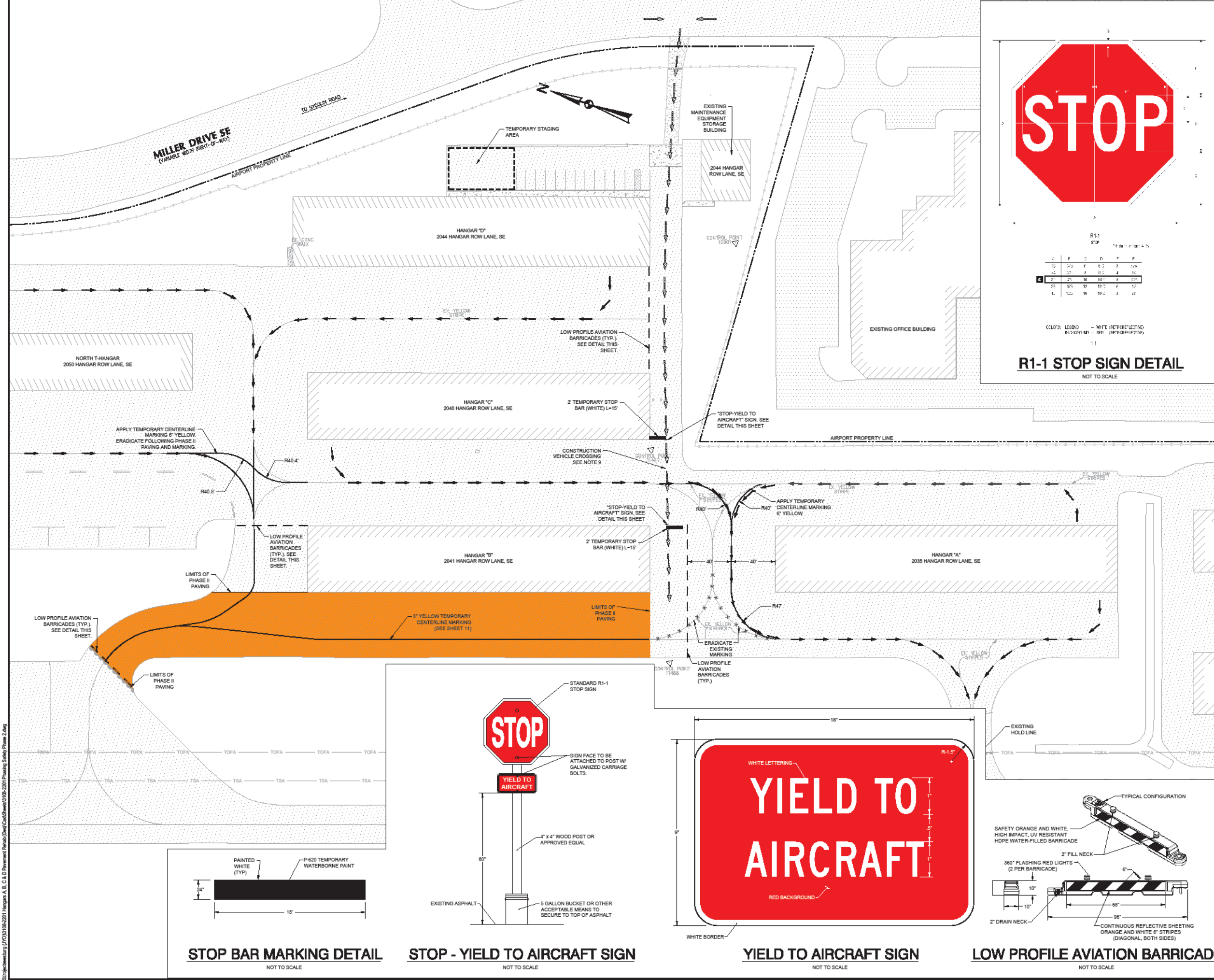
REV.	DESCRIPTION	DATE

PHASING & SAFETY PLAN - PHASE 2
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	1"=40'
Drawn	REX
Checked	STP
Project No.	3108-2201
Sheet No.	5
Of	16



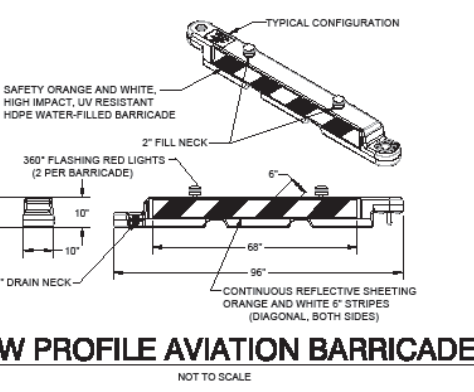
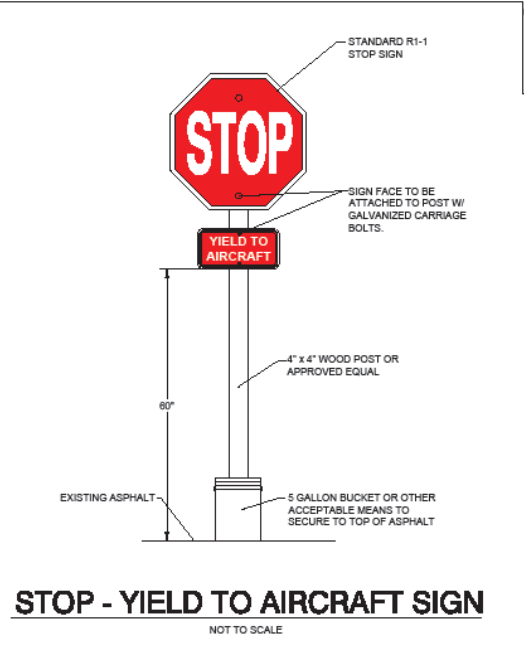
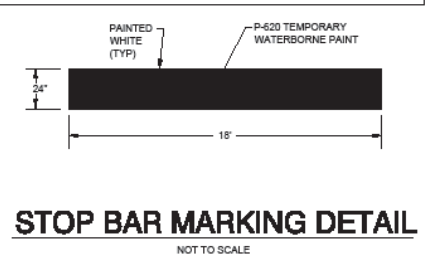
	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT	[Symbol]	[Symbol]
TAXIWAY OBJECT FREE AREA	[Symbol]	[Symbol]
TAXIWAY SAFETY AREA	[Symbol]	[Symbol]
LOW PROFILE AVIATION BARRICADE	[Symbol]	[Symbol]
PHASE 1 WORK AREA	[Symbol]	[Symbol]
AIRCRAFT TAXI ROUTE	[Symbol]	[Symbol]
TIE-DOWN	[Symbol]	[Symbol]
BUILDING	[Symbol]	[Symbol]
HAUL ROUTE	[Symbol]	[Symbol]
PROPERTY LINE	[Symbol]	[Symbol]
CONTROL POINT	[Symbol]	[Symbol]
FENCE	[Symbol]	[Symbol]
GRAVEL	[Symbol]	[Symbol]

- AVIATION BARRICADE NOTES:**
1. THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 2. LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE. LIGHTS SHALL BE RED.
 3. EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 4. CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 5. BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP.)

- PHASING NOTES:**
- PHASE 2**
1. PHASE 2 WORK AREA WILL IMPACT THE FOLLOWING:
EAST SIDE OF HANGAR 'B'
- THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
2. PLACE LOW PROFILE BARRICADES AS PER PLAN.
 3. ERADICATE THE EXISTING CENTERLINE MARKING AS REQUIRED. PLACE TEMPORARY CENTERLINE MARKING, STOP BARS, AND SIGNS.
 4. PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES WITHIN PHASE 2 LIMITS AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 2 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 5. PERFORM FULL DEPTH RECLAMATION, CONCRETE IMPROVEMENTS AND ASPHALT PLACEMENT.
 6. PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 2 AREA ONLY.
 7. WORK IN PHASE 2 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO CLOSING OF PHASE 3 WORK AREA.
 8. PHASE 2 CONTRACT TIME 10 CALENDAR DAYS.
 9. THE CONTRACTOR IS ADVISED THAT THE EXISTING PAVEMENT BETWEEN HANGAR B & C WILL REMAIN OPERATIONAL AND AIRCRAFT TAXIING WILL CONTINUE DURING PHASE 2 WORK. THE CONTRACTOR SHALL ADVISE ALL PERSONNEL (INCLUDING SUBS) OF THE REQUIREMENT TO STOP AND CONFIRM NO AIRCRAFT ARE TAXIING THROUGH THE AREA AT THE TIME OF CROSSING. ALL CONSTRUCTION EQUIPMENT SHALL YIELD TO AIRCRAFT AT ALL TIMES. DURING HAULING OPERATIONS, THE CONTRACTOR SHALL POST A COMPETENT EMPLOYEE AT THE INTERSECTION TO DIRECT CONSTRUCTION TRAFFIC TO ENSURE PLANES ARE NOT TAXIING AT THE TIME OF VEHICLE CROSSING.
- AS PART OF THE CONTRACTOR'S PHASING AND SCHEDULE PREPARATION, SCHEDULING BETWEEN THE CONTRACTOR/OWNER/TENANT WILL BE REQUIRED ON A WEEKLY BASIS TO ENSURE MINIMAL IMPACT TO THE TENANTS AND CONTRACTOR ALIKE. NO MATERIAL, REGARDLESS OF NATURE, SHALL BE STOCKPILED ADJACENT TO THE TAXILANE OBJECT FREE AREA (TOFA). EXISTING ASPHALT SHALL BE PROTECTED AT ALL TIMES.

SHEET INDEX

DATE: FEBRUARY 2023
SCALE: 1"=40'
DRAWN: REK
CHECKED: STP
PROJECT NO.: 3108-2201
SHEET NO.: 5
TOTAL SHEETS: 16



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1005 KRAUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23832
PHONE: 804-6868678 FAX: 804-6868671

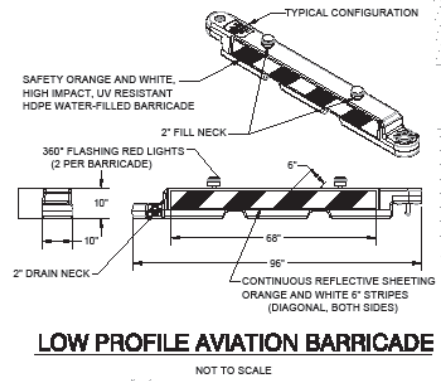
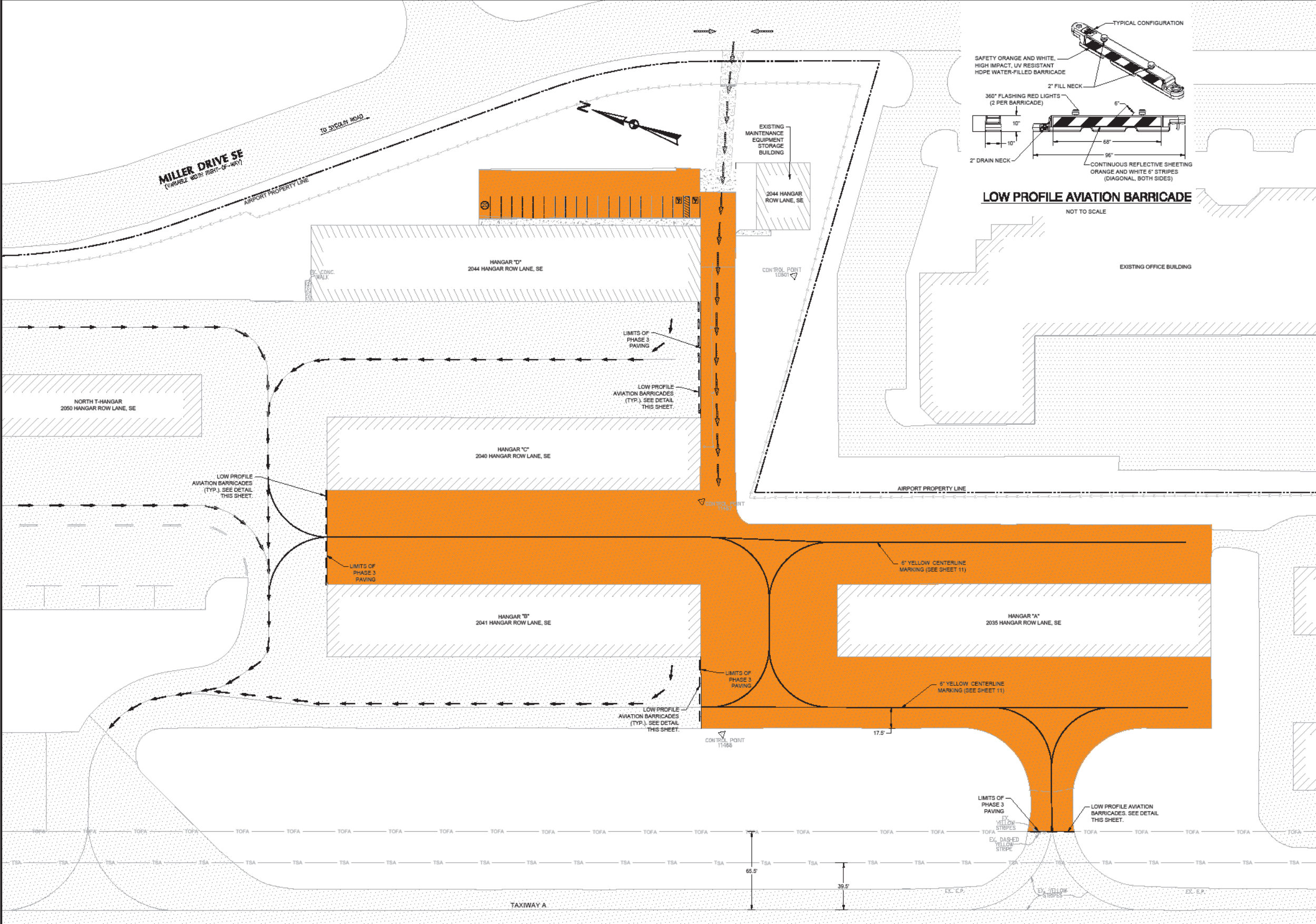
PHASING & SAFETY PLAN - PHASE 2
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.

COMMISSIONED PROFESSIONAL ENGINEER
STEVEN T. PETERSON
Lic. No. 038457
2-16-23

Date: FEBRUARY 2023
Scale: 1"=40'
Drawn: REK
Checked: STP
Project No.: 3108-2201
Sheet No.: 5
Total Sheets: 16

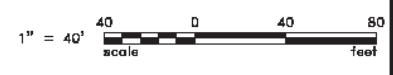
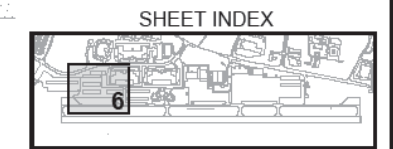
Storck-Bennett (1/17/2023) Hangars A, B, C & D Pavement Rehabilitation (Plan) - 2021 Phasing Safety Phase 3.dwg



	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT	[Symbol]	[Symbol]
TAXIWAY OBJECT FREE AREA	[Symbol]	[Symbol]
TAXIWAY SAFETY AREA	[Symbol]	[Symbol]
LOW PROFILE AVIATION BARRICADE	[Symbol]	[Symbol]
PHASE 1 WORK AREA	[Symbol]	[Symbol]
AIRCRAFT TAXI ROUTE	[Symbol]	[Symbol]
TIE-DOWN	[Symbol]	[Symbol]
BUILDING	[Symbol]	[Symbol]
HAUL ROUTE	[Symbol]	[Symbol]
PROPERTY LINE	[Symbol]	[Symbol]
CONTROL POINT	[Symbol]	[Symbol]
FENCE	[Symbol]	[Symbol]
GRAVEL	[Symbol]	[Symbol]

- AVIATION BARRICADE NOTES:**
- THE AIRPORT WILL NOT PROVIDE AVIATION TYPE BARRICADES. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AVIATION BARRICADES IN SUFFICIENT QUANTITIES TO COMPLETE THE WORK FOR THIS CONTRACT.
 - LOW PROFILE AVIATION BARRICADES SHALL BE PRE-MADE COMMERCIALY AVAILABLE UNITS, OR ASSEMBLED BY THE CONTRACTOR. SHOP DRAWINGS ARE REQUIRED. COLOR TO BE AVIATION ORANGE AND WHITE, LIGHTS SHALL BE RED.
 - EXTENDED BARRICADES ACROSS THE FULL WIDTH OF THE PAVEMENT. PROVIDE HOLD DOWN AND LATERAL SUPPORT TO PREVENT OVERTURNING FROM PROP WASH AND JET BLAST, WING VORTEX, OR OTHER SURFACE WIND CURRENTS. BARRICADES TO BE PLACED FULL WIDTH ACROSS CLOSED SECTIONS.
 - CONTRACTOR SHALL MONITOR LIGHTED BARRICADES ON A REGULAR BASIS TO MAKE SURE LIGHTS ARE OPERATIONAL. THE CONTRACTOR SHALL MAINTAIN A SUPPLY OF BATTERIES AND LIGHTS TO REPLACE AS REQUIRED TO MAINTAIN LIGHTS IN OPERATION AT ALL TIMES.
 - BARRICADES SHALL BE POSITIONED AS SHOWN IN THE PLANS. BARRICADES ARE SPACED AT 6' MAX. (TYP.)

- PHASING NOTES:**
- PHASE 3**
- RELOCATE LOW PROFILE BARRICADES TO THE LOCATIONS DEPICTED FOR PHASE 3 WORK AREA.
 - PHASE 3 WORK AREA WILL IMPACT THE FOLLOWING:
HANGAR "A"
WEST SIDE OF HANGAR "B"
THE CONTRACTOR SHALL GIVE THE OWNER'S PROJECT MANAGER 14 CALENDAR DAYS NOTICE PRIOR TO BEGINNING WORK IN THIS AREA TO ALLOW TIME TO COORDINATE WITH THE AIRPORT TENANTS.
 - PLACE LOW PROFILE BARRICADES AS PER PLAN.
 - ERADICATE THE EXISTING CENTERLINE MARKING AS REQUIRED.
 - PERFORM MISCELLANEOUS DEMOLITION ACTIVITIES WITHIN AREA 3 LIMITS AS PER PLAN. THE CONTRACTOR SHALL ASSESS THE PLAN AND DEVELOP A SCHEDULE FOR IMPLEMENTATION. NO CONSTRUCTION ACTIVITIES SHALL BE PERFORMED OUTSIDE OF THE PHASE 3 AREA THAT WOULD IMPACT AIRPORT OPERATIONS.
 - PERFORM FULL DEPTH RECLAMATION AND ASPHALT PLACEMENT.
 - PLACE TEMPORARY CENTERLINE MARKING WITHIN THE PHASE 3 AREA ONLY.
 - WORK IN PHASE 3 AREA SHALL BE COMPLETE AND ACCEPTED BY THE OWNER'S PROJECT MANAGER PRIOR TO BARRICADE REMOVAL.
 - 30 DAYS AFTER FINAL PAVING IN THE PHASE 3 AREA, PLACE PERMANENT CENTERLINE MARKINGS ON ALL PHASE 1, 2 & 3 AREAS. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER'S PROJECT MANAGER.
 - PHASE 3 CONTRACT TIME - 20 CALENDAR DAYS.



TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1005 KRAUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23832
PHONE: 804-686-6676 FAX: 804-686-6671

REV.	DESCRIPTION	DATE

PHASING & SAFETY PLAN - PHASE 3
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	1"=40'
Drawn	REX
Checked	STP
Project No.	3108-2201
Sheet No.	6
Of	16



MILLER DRIVE SE
(VEHICLE WIDTH RIGHT-OF-WAY)

INSTALL RUBBER CURB RAMP FOR ACCESS OVER EXISTING CONCRETE CURB AND GUTTER. NO SEPARATE PAY ITEM. SEE NOTE 8 ON SHEET 8, SITE AND MARKING PLAN.

EXISTING GATE TO BE USED FOR TEMPORARY CONSTRUCTION ACCESS. SEE NOTE 13.

CONTRACTOR TO EVALUATE AND IMPROVE EXISTING GRAVEL ACCESS AS NEEDED. NO SEPARATE PAY ITEM.

DEMOLISH AND REMOVE EXISTING CONCRETE PATCH

PROTECT EXISTING WATER VALVES (TYP.) SEE PHOTO SHEET 15.

PROTECT EXISTING ASPHALT AND CONCRETE

EXISTING MAINTENANCE EQUIPMENT STORAGE BUILDING

2044 HANGAR ROW LANE, SE

HANGAR 'D'
2044 HANGAR ROW LANE, SE

PROTECT EXISTING SEWER MANHOLE

NEAT VERTICAL SAWCUT, TACK JOINT PRIOR TO PAVING.

PROTECT EXISTING CLEANOUT

EXISTING OFFICE BUILDING

NEAT VERTICAL SAWCUT, TACK JOINT PRIOR TO PAVING.

NEAT VERTICAL SAWCUT, RECLAMATION LIMITS.

INLET SPALL REPAIR SEE PHOTO SHEET 15.

FULL DEPTH RECLAMATION

PROTECT THE EDGES OF EXISTING TRENCH DRAIN

DEMOLISH AND REMOVE EXISTING CONCRETE PATCH (2' x 2' x 4')

EXISTING ELECTRICAL METER, SEE NOTE 11.

EXISTING ROOF DRAIN (TYP.) SEE PHOTO SHEET 15.

HANGAR 'C'
2040 HANGAR ROW LANE, SE

EXISTING ROOF DRAIN (TYP.) SEE PHOTO SHEET 15.

PROTECT EXISTING CLEANOUTS (TYP.) SEE PHOTO SHEET 15.

EXISTING DOORS. PROTECT THRESHOLDS AT ALL TIMES.

AIRPORT PROPERTY LINE

MILL EXISTING ASPHALT, DEPTH - 2"

NEAT VERTICAL SAW CUT

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

NEAT VERTICAL SAWCUT, RECLAMATION LIMITS.

SPALL REPAIR
AFTER RECLAMATION HAS BEEN COMPLETED AROUND THE TRENCH DRAINS, THE CONTRACTOR, ALONG WITH THE OWNER'S PROJECT MANAGER, SHALL INSPECT THE TRENCH DRAIN EDGES TO CONFIRM AND MARK THE FINAL AREAS OF SPALL REPAIR. OBVIOUS DAMAGE CREATED BY THE CONTRACTOR DURING RECLAMATION/DEMOLITION SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

CONTROL POINT 11485

FULL DEPTH RECLAMATION

FULL DEPTH RECLAMATION

NEAT VERTICAL SAWCUT, RECLAMATION LIMITS.

NEAT VERTICAL SAWCUT, TACK JOINT PRIOR TO PAVING.

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TOFA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TSA

TAXIWAY A

66.5'

39.5'

EX. EP

EX. EP



LEGEND		
EXISTING	PROPOSED	
[Symbol]	[Symbol]	BITUMINOUS PAVEMENT
[Symbol]	[Symbol]	CONCRETE
[Symbol]	[Symbol]	TAXIWAY OBJECT FREE AREA
[Symbol]	[Symbol]	TAXIWAY SAFETY AREA
[Symbol]	[Symbol]	TAXIWAY SAFETY AREA
[Symbol]	[Symbol]	GAS LINE
[Symbol]	[Symbol]	ELECTRIC LINE
[Symbol]	[Symbol]	SANITARY SEWER LINE
[Symbol]	[Symbol]	WATER LINE
[Symbol]	[Symbol]	TELEPHONE LINE
[Symbol]	[Symbol]	BUILDING
[Symbol]	[Symbol]	CLEAN OUT
[Symbol]	[Symbol]	CATCH BASIN
[Symbol]	[Symbol]	16" FULL DEPTH RECLAMATION
[Symbol]	[Symbol]	PAVEMENT MILLING
[Symbol]	[Symbol]	BORING LOCATION
[Symbol]	[Symbol]	PROPERTY LINE
[Symbol]	[Symbol]	CONTROL POINT
[Symbol]	[Symbol]	FENCE
[Symbol]	[Symbol]	GRAVEL

- DEMOLITION NOTES**
- THE CONTRACTOR SHALL PROTECT AND PRESERVE THE EXISTING SURVEY BENCHMARK MONUMENTS THROUGHOUT THE DURATION OF THE PROJECT. ANY MONUMENTS/CONTROL DAMAGED SHALL BE REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 - EXISTING HANGARS, FENCING, AND ELECTRICAL EQUIPMENT SHALL BE PROTECTED BY THE CONTRACTOR AGAINST DAMAGE FROM CONSTRUCTION EQUIPMENT AND OPERATIONS. ANY DAMAGE DONE TO AIRPORT FACILITIES FOR ITEMS NOT NOTED TO BE DEMOLISHED OR REMOVED SHALL BE REPAIRED BY THE CONTRACTOR IMMEDIATELY, TO AIRPORT MANAGEMENT'S SATISFACTION, AT THE CONTRACTOR'S EXPENSE.
 - EXISTING ASPHALT/CONCRETE SHALL BE REMOVED AND DISPOSED OF OFF-SITE. THE CONTRACTOR SHALL PROTECT THE EXISTING SUBGRADE MATERIAL FROM BECOMING UNSTABLE DUE TO CONSTRUCTION TRAFFIC. THIS INCLUDES PROPER SCHEDULING SO AS NOT TO REMOVE THE ASPHALT/CONCRETE AND EXPOSE BASE AND SUB-BASE MATERIAL TO INCREMENT WEATHER.
 - FOR ALL DEMOLITION, THE CONTRACTOR SHALL ASSESS THE DETAILS WITH REGARDS TO SCHEDULE AND HOW HE ANTICIPATES WORKING THE SITE BASED ON THE PHASING PLAN. THE CONTRACTOR SHALL PROVIDE A SCHEDULE TO AIRPORT MANAGEMENT AT LEAST 14 DAYS PRIOR TO THE START OF DEMOLITION IN ORDER TO ALLOW AIRPORT MANAGEMENT ENOUGH TIME TO ASSESS THE IMPACTS THE SCHEDULE WILL HAVE TO AIRPORT OPERATIONS. CHANGES TO THE SCHEDULE WILL BE MADE SHOULD THE PROPOSED SCHEDULE CREATE A SIGNIFICANT IMPACT TO OPERATIONS.
 - THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM AS-BUILT DRAWINGS AND VISUAL FIELD LOCATION METHODS. NEITHER AIRPORT MANAGEMENT OR THE OWNER'S PROJECT MANAGER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION SHOWN. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZES AND TYPES OF MATERIALS OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES INCLUDING HOMERUN CABLES AND SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OWNER'S PROJECT MANAGER OF HIS OPERATIONAL PLANS. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S PROJECT MANAGER. ANY SUCH SERVICES DISTURBED BY THE CONTRACTOR'S OPERATION SHALL BE RESTORED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - THE LOCATION OF THE EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR IS TO EXERCISE CAUTION AND USE HAND EXCAVATION TOOLS FOR ANY EXCAVATION IN THE VICINITY OF ANY EXISTING INSTALLATIONS. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. ALL DISTURBED AREAS OUTSIDE OF THE EDGES OF PAVEMENT ARE TO BE RESTORED TO THE ORIGINAL SURFACE ELEVATION, RAKED SMOOTH AND SEEDED.
 - ALL ITEMS NOTED IN THE PLANS TO BE DEMOLISHED OR REMOVED SHALL BE PAID FOR UNDER THE DEMOLITION LINE ITEM.
 - SEE PHASING & SAFETY PLAN SHEETS FOR LOCATION OF REQUIRED BARRICADES AND SEQUENCING OF PROJECT REQUIREMENTS.
 - SEE SHEETS 12, 13 & 14 FOR BORING RESULTS.
 - THE CONTRACTOR SHALL ENSURE ALL GRASS AND DEBRIS ARE REMOVED FROM ALL JOINTS (CONCRETE OR ASPHALT) WITHIN THE PROJECT LIMITS.
 - THE CONTRACTOR SHALL PROTECT ALL ROOF DRAINS, ELECTRICAL PANELS/METERS, GAS METERS AND CONDUIT STUBS DURING DEMOLITION AND/OR RECLAMATION. DAMAGE TO TOWN PROPERTY SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. ANY EXISTING DAMAGE SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER'S PROJECT MANAGER PRIOR TO THE START OF DEMOLITION.
 - THE CONTRACTOR SHALL PROTECT THE EDGES OF ALL EXISTING CONCRETE INCLUDING, BUT NOT LIMITED TO, TRENCH DRAIN DROP INLETS, BUILDINGS, CURB & GUTTER, CONCRETE PADS AND SIDEWALKS. ANY DAMAGE SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
 - THE CONTRACTOR SHALL SECURE THE EXISTING GATE AT THE END OF EACH DAY'S WORK. THE CONTRACTOR WILL BE RESPONSIBLE FOR MONITORING THE GATE WHEN THE GATE IS OPEN SO AS NOT TO ALLOW UNAUTHORIZED ENTRY OF VEHICLES OR INDIVIDUALS. UNAUTHORIZED ACCESS SHALL BE REPORTED TO AIRPORT MANAGEMENT IMMEDIATELY.

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1005 KRAUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23033
PHONE: 804-666-8676 FAX: 804-666-8671

REV	DESCRIPTION	DATE

EXISTING CONDITIONS & DEMOLITION
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	1" = 40'
Drawn	REX
Checked	STP
Project No.	3108-2201
Sheet No.	7
Of	16

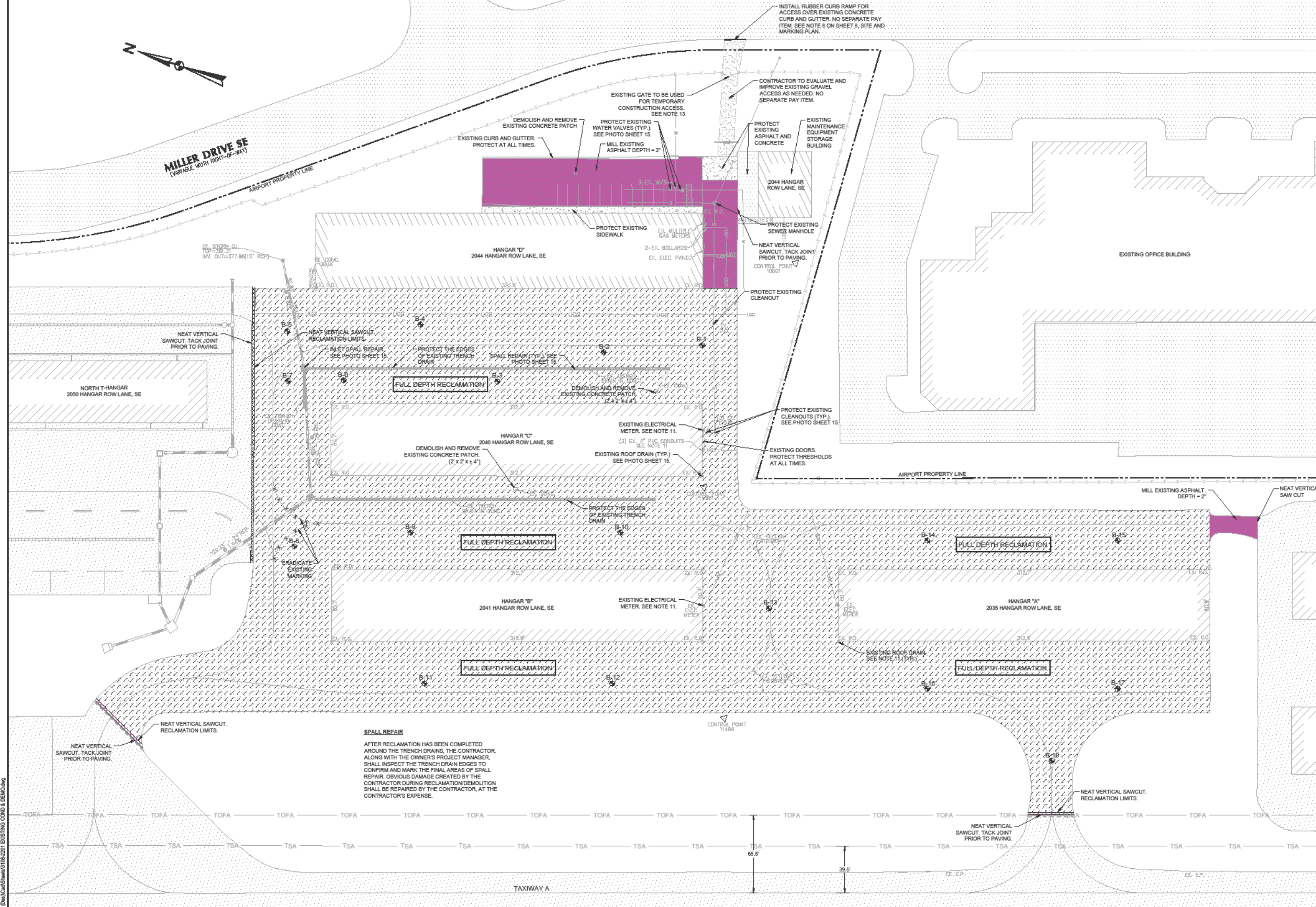
SHEET INDEX

3108-2201-07.dwg 2/1/23 10:16:25AM C:\Users\shelby1618\OneDrive\Documents\3108-2201-07.dwg



MILLER DRIVE SE
(VARIABLE WIDTH RIGHT-OF-WAY)

AIRPORT PROPERTY LINE



	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT		
CONCRETE		
TAXIWAY OBJECT FREE AREA	TOFA	
TAXIWAY SAFETY AREA	TSA	
GAS LINE	UGD	
ELECTRIC LINE	UGE	
SANITARY SEWER LINE	US	
WATER LINE	UW	
TELEPHONE LINE	UGT	
BUILDING		
CLEAN OUT		
CATCH BASIN		
16' FULL DEPTH RECLAMATION		
PAVEMENT MILLING		
BORING LOCATION		
PROPERTY LINE		
CONTROL POINT		
FENCE		
GRAVEL		

- DEMOLITION NOTES:**
- THE CONTRACTOR SHALL PROTECT AND PRESERVE THE EXISTING SURVEY BENCHMARK MONUMENTS THROUGHOUT THE DURATION OF THE PROJECT. ANY MONUMENTS/CONTROL DAMAGED SHALL BE REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
 - EXISTING HANGARS, FENCING, AND ELECTRICAL EQUIPMENT SHALL BE PROTECTED BY THE CONTRACTOR AGAINST DAMAGE FROM CONSTRUCTION EQUIPMENT AND OPERATIONS. ANY DAMAGE DONE TO AIRPORT FACILITIES FOR ITEMS NOT NOTED TO BE DEMOLISHED OR REMOVED SHALL BE REPAIRED BY THE CONTRACTOR IMMEDIATELY, TO AIRPORT MANAGEMENT'S SATISFACTION, AT THE CONTRACTOR'S EXPENSE.
 - EXISTING ASPHALT/CONCRETE SHALL BE REMOVED AND DISPOSED OF OFF-SITE. THE CONTRACTOR SHALL PROTECT THE EXISTING SUBGRADE MATERIAL FROM BECOMING UNSTABLE DUE TO CONSTRUCTION TRAFFIC. THIS INCLUDES PROPER SCHEDULING SO AS NOT TO REMOVE THE ASPHALT/CONCRETE AND EXPOSE BASE AND SUB-BASE MATERIAL TO INCLEMENT WEATHER.
 - FOR ALL DEMOLITION, THE CONTRACTOR SHALL ASSESS THE DETAILS WITH REGARD TO SCHEDULE AND HOW HE ANTICIPATES WORKING THE SITE BASED ON THE PHASING PLAN. THE CONTRACTOR SHALL PROVIDE A SCHEDULE TO AIRPORT MANAGEMENT AT LEAST 14 DAYS PRIOR TO THE START OF DEMOLITION IN ORDER TO ALLOW AIRPORT MANAGEMENT ENOUGH TIME TO ASSESS THE IMPACTS THE SCHEDULE WILL HAVE TO AIRPORT OPERATIONS. CHANGES TO THE SCHEDULE WILL BE MADE SHOULD THE PROPOSED SCHEDULE CREATE A SIGNIFICANT IMPACT TO OPERATIONS.
 - THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM AS-BUILT DRAWINGS AND VISUAL FIELD LOCATION METHODS. NEITHER AIRPORT MANAGEMENT OR THE OWNER'S PROJECT MANAGER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION SHOWN. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZES AND TYPES OF MATERIALS OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES INCLUDING HOMERUN CABLES AND SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OWNER'S PROJECT MANAGER OF HIS OPERATIONAL PLANS. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S PROJECT MANAGER. ANY SUCH SERVICES DISTURBED BY THE CONTRACTOR'S OPERATION SHALL BE RESTORED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - THE LOCATION OF THE EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR IS TO EXERCISE CAUTION AND USE HAND EXCAVATION TOOLS FOR ANY EXCAVATION IN THE VICINITY OF ANY EXISTING INSTALLATIONS. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. ALL DISTURBED AREAS OUTSIDE OF THE EDGES OF PAVEMENT ARE TO BE RESTORED TO THE ORIGINAL SURFACE ELEVATION, RAKED SMOOTH AND SEEDED.
 - ALL ITEMS NOTED IN THE PLANS TO BE DEMOLISHED OR REMOVED SHALL BE PAID FOR UNDER THE DEMOLITION LINE ITEM.
 - SEE PHASING & SAFETY PLAN SHEETS FOR LOCATION OF REQUIRED BARRICADES AND SEQUENCING OF PROJECT REQUIREMENTS.
 - SEE SHEETS 12, 13 & 14 FOR BORING RESULTS.
 - THE CONTRACTOR SHALL ENSURE ALL GRASS AND DEBRIS ARE REMOVED FROM ALL JOINTS (CONCRETE OR ASPHALT) WITHIN THE PROJECT LIMITS.
 - THE CONTRACTOR SHALL PROTECT ALL ROOF DRAINS, ELECTRICAL PANELS/METERS, GAS METERS AND CONDUIT STUBS DURING DEMOLITION AND/OR RECLAMATION. DAMAGE TO TOWN PROPERTY SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. ANY EXISTING DAMAGE SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER'S PROJECT MANAGER PRIOR TO THE START OF DEMOLITION.
 - THE CONTRACTOR SHALL PROTECT THE EDGES OF ALL EXISTING CONCRETE INCLUDING, BUT NOT LIMITED TO, TRENCH DRAIN DROP INLETS, BUILDINGS, CURB & GUTTER, CONCRETE PADS AND SIDEWALKS. ANY DAMAGE SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
 - THE CONTRACTOR SHALL SECURE THE EXISTING GATE AT THE END OF EACH DAY'S WORK. THE CONTRACTOR WILL BE RESPONSIBLE FOR MONITORING THE GATE WHEN THE GATE IS OPEN SO AS NOT TO ALLOW UNAUTHORIZED ENTRY OF VEHICLES OR INDIVIDUALS. UNAUTHORIZED ACCESS SHALL BE REPORTED TO AIRPORT MANAGEMENT IMMEDIATELY.

SPALL REPAIR
AFTER RECLAMATION HAS BEEN COMPLETED AROUND THE TRENCH DRAINS, THE CONTRACTOR, ALONG WITH THE OWNER'S PROJECT MANAGER, SHALL INSPECT THE TRENCH DRAIN EDGES TO CONFIRM AND MARK THE FINAL AREAS OF SPALL REPAIR. OBVIOUS DAMAGE CREATED BY THE CONTRACTOR DURING RECLAMATION/DEMOLITION SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

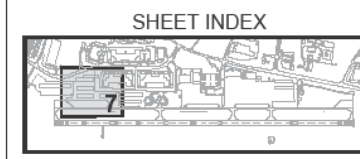
TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1008 KRAUSE ROAD, SUITE 100
CHURCHFIELD, VIRGINIA 23832
PHONE: 804/768-8878 FAX: 804/768-8871

EXISTING CONDITIONS & DEMOLITION
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

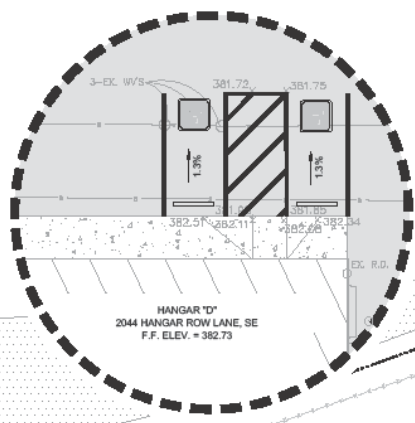
Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	1"=40'
Drawn	REX
Checked	STP
Project No.	3108-2201
Sheet No.	7
Of	16



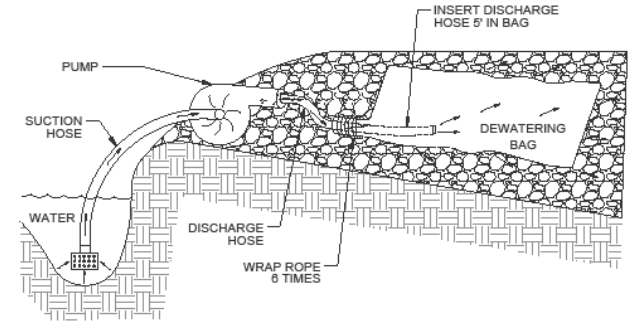
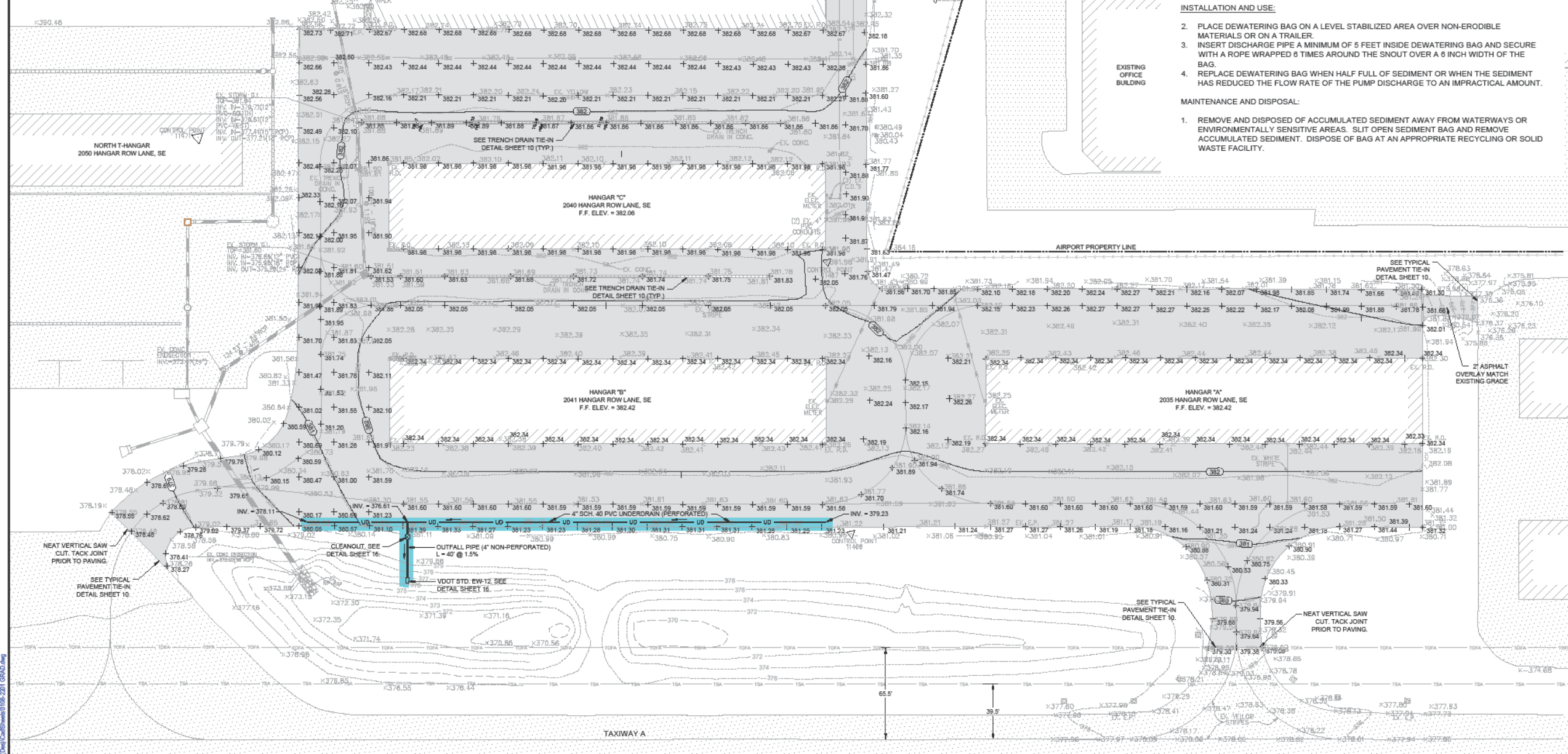
S:\projects\3108-2201\3108-2201_Hangars A, B, C & D Pavement Rehabilitation\Drawings\3108-2201-EXISTING COND 1.DWG



MILLER DRIVE SE
(VARIABLE 40 FT. RIGHT-OF-WAY)

AIRPORT PROPERTY LINE

INSET "A"



DEWATERING STRUCTURE/BAG
NOT TO SCALE

- NOTE:**
1. A SEDCATCH DEWATERING BAG OR APPROVED EQUAL SHOULD BE USED ANYTIME WATER IS PUMPED ON THE SITE.
- INSTALLATION AND USE:**
2. PLACE DEWATERING BAG ON A LEVEL STABILIZED AREA OVER NON-ERODIBLE MATERIALS OR ON A TRAILER.
 3. INSERT DISCHARGE PIPE A MINIMUM OF 5 FEET INSIDE DEWATERING BAG AND SECURE WITH A ROPE WRAPPED 8 TIMES AROUND THE SNOOT OVER A 6 INCH WIDTH OF THE BAG.
 4. REPLACE DEWATERING BAG WHEN HALF FULL OF SEDIMENT OR WHEN THE SEDIMENT HAS REDUCED THE FLOW RATE OF THE PUMP DISCHARGE TO AN IMPRACTICAL AMOUNT.
- MAINTENANCE AND DISPOSAL:**
1. REMOVE AND DISPOSED OF ACCUMULATED SEDIMENT AWAY FROM WATERWAYS OR ENVIRONMENTALLY SENSITIVE AREAS. SLIT OPEN SEDIMENT BAG AND REMOVE ACCUMULATED SEDIMENT. DISPOSE OF BAG AT AN APPROPRIATE RECYCLING OR SOLID WASTE FACILITY.

	LEGEND	
	EXISTING	PROPOSED
BITUMINOUS PAVEMENT	[Symbol]	[Symbol]
CONCRETE	[Symbol]	[Symbol]
TAXIWAY OBJECT FREE AREA	[Symbol]	[Symbol]
TAXIWAY SAFETY AREA	[Symbol]	[Symbol]
GAS LINE	[Symbol]	[Symbol]
ELECTRIC LINE	[Symbol]	[Symbol]
SANITARY SEWER LINE	[Symbol]	[Symbol]
WATER LINE	[Symbol]	[Symbol]
TELEPHONE LINE	[Symbol]	[Symbol]
BUILDING	[Symbol]	[Symbol]
CLEAN OUT	[Symbol]	[Symbol]
CATCH BASIN	[Symbol]	[Symbol]
PROPERTY LINE	[Symbol]	[Symbol]
CONTROL POINT	[Symbol]	[Symbol]
FENCE	[Symbol]	[Symbol]
GRAVEL	[Symbol]	[Symbol]
UNDERDRAIN	[Symbol]	[Symbol]

NOTES:

1. THE CONTRACTOR SHALL CONFIRM THE PERIMETER OF ALL DRAINAGE STRUCTURES, WATER VALVES AND CLEANOUTS ARE FULLY COMPACTED PRIOR TO THE PLACEMENT OF STONE/ASPHALT.
2. THE CONTRACTOR IS ADVISED THAT THE PAVING OF THE APRON WILL BE CONDUCTED IN THREE PHASES. THE CONTRACTOR SHALL ANTICIPATE THE PLACEMENT OF WEDGE TRANSITIONS FOR CONSTRUCTION VEHICLES AND FOR GENERAL MOVEMENT OF CONSTRUCTION EQUIPMENT ON SITE. SAW CUTTINGS OF ALL JOINTS AS REQUIRED BY SPECIFICATION OR NOTED IN THE PLANS IS INCIDENTAL TO THE UNITS BID UPON.
3. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE PROPERLY SIZED PUMPS, HOSES AND ALL APPURTENANCES NECESSARY TO SUCCESSFULLY DEWATER THROUGHOUT THE DURATION OF THE PROJECT. MANAGING SITE DRAINAGE WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

THE CONTRACTOR IS REQUIRED TO PROVIDE THE PROPER SEDIMENT AND EROSION CONTROLS FOR DEWATERING OPERATIONS AND SHALL BE CONSISTENT WITH THE REQUIREMENTS ESTABLISHED UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM REQUIREMENTS. THIS SHALL INCLUDE PROVIDING GEOTEXTILE BAGS (SEE DETAIL THIS SHEET) MADE OF PERMEABLE, NONWOVEN GEOTEXTILES TO BE FITTED TO THE ENDS OF EACH PUMP AS REQUIRED. DEWATERING IS INCIDENTAL TO THE WORK.

TALBERT & BRIGHT
ENGINEERING & PLANNING CONSULTANTS
1005 KRAUSE ROAD, SUITE 100
CHESTERFIELD, VIRGINIA 23832
PHONE: 804-768-6878 FAX: 804-768-6871

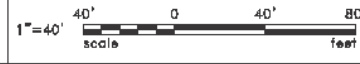
GRADING & PAVING PLAN
Hangars A, B, C & D Pavement Rehabilitation
DOAV No. CS0027-45
Leesburg Executive Airport - Leesburg, Virginia

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.

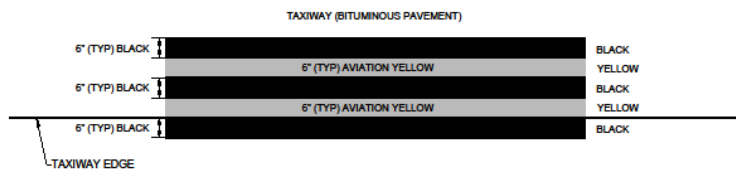


SHEET INDEX

Date	FEBRUARY 2023
Scale	1"=40'
Drawn	REK
Checked	STP
Project No.	3108-2201
Sheet No.	9
Of	16

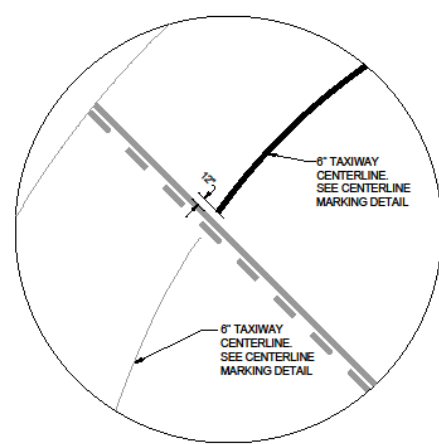


S:\Information\2023\3108-2201\Hangars A, B, C & D Pavement Rehab (Detail)\CS0027-45-2201-GRD.dwg



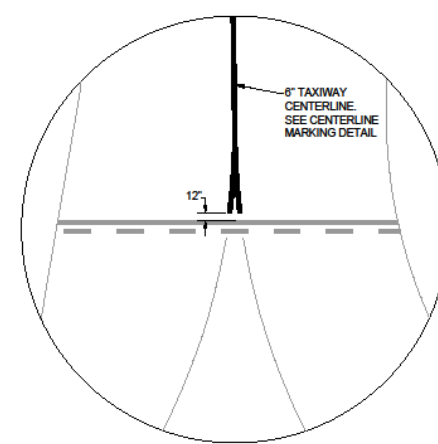
TAXIWAY EDGE MARKING DETAIL

NOT TO SCALE



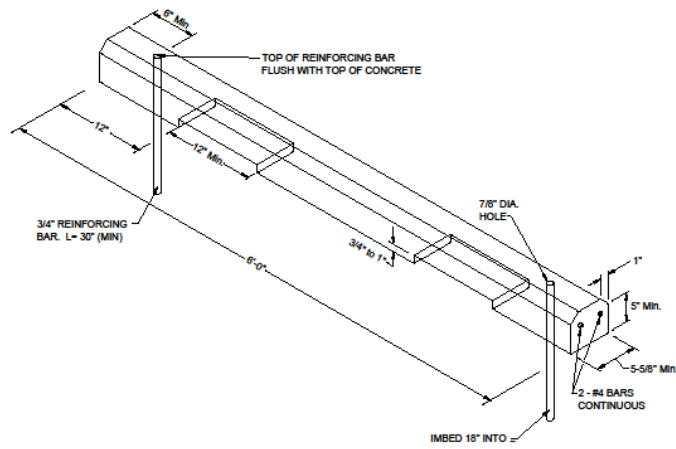
HOLD LINE MARKING #1 DETAIL

NOT TO SCALE



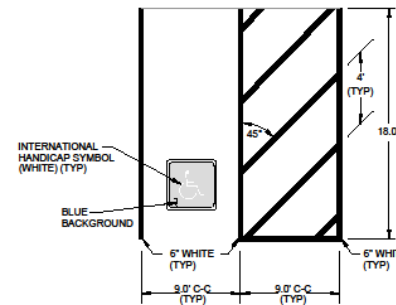
HOLD LINE MARKING #2 DETAIL

NOT TO SCALE



BUMPER BLOCK DETAIL

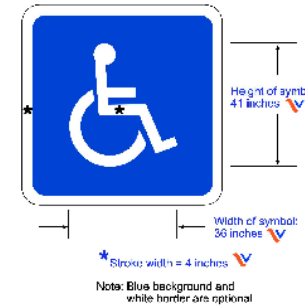
NOT TO SCALE



TYPICAL HANDICAPPED PARKING LAYOUT

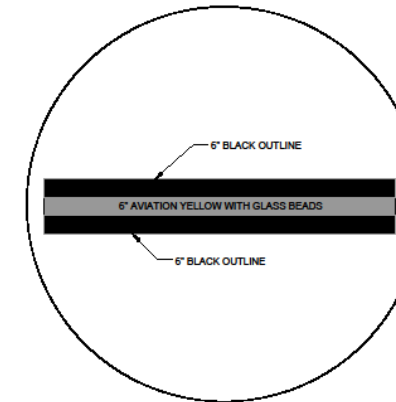
NOT TO SCALE

Figure 3B-22(VA). International Symbol of Accessibility Parking Space Marking



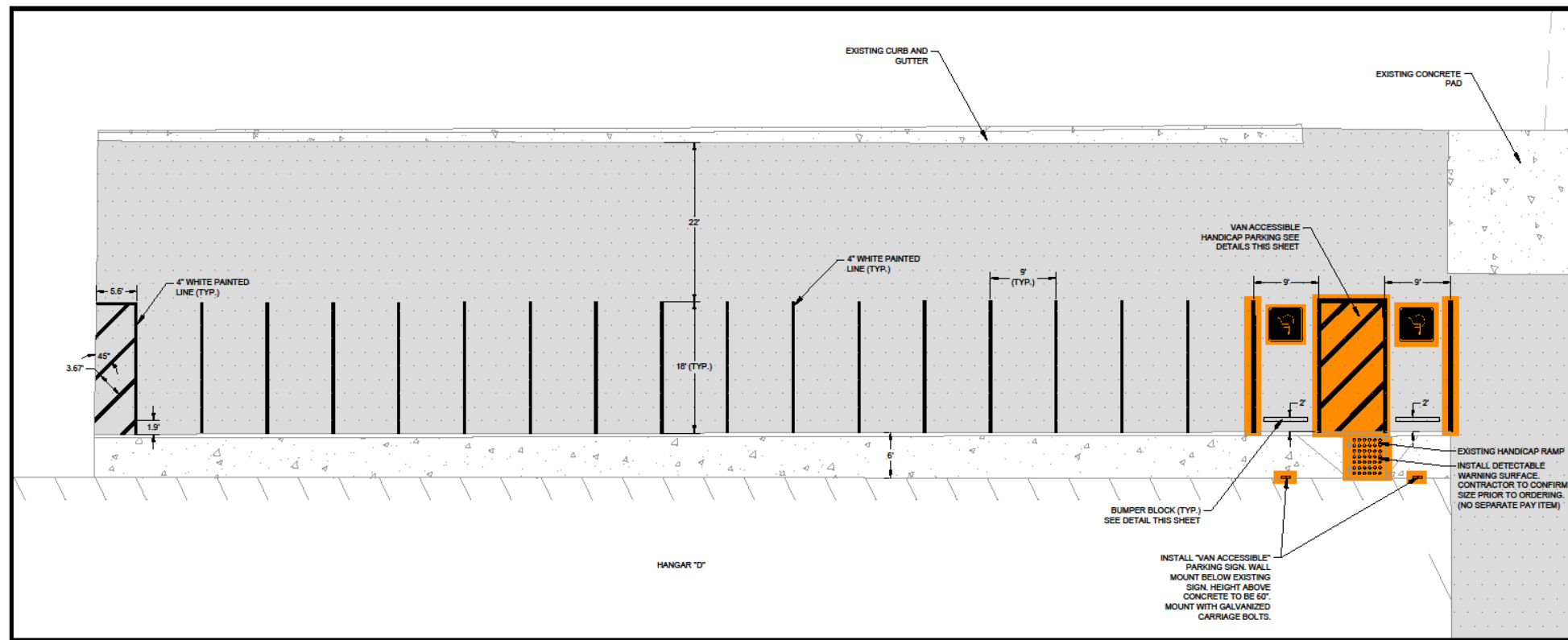
HANDICAPPED PARKING PAVEMENT SYMBOL

NOT TO SCALE



CENTERLINE MARKING

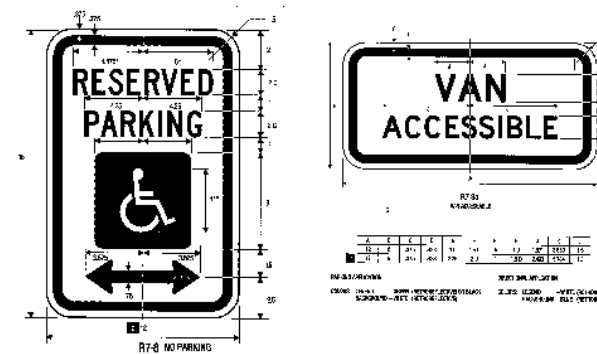
NOT TO SCALE



PARKING LOT MARKING & SIGNAGE DETAIL

SCALE 1" = 10'

NOTE:
ALL MARKING SHALL BE APPLIED MEETING VDOT SPECIFICATION VA-704. PAVEMENT MARKINGS SHALL CONFORM TO VDOT SECTION 248, AND GLASS BEAD AND RETROREFLECTIVE OPTICS MATERIALS SHALL CONFORM TO VDOT SECTION 234.



LEGEND - GREEN (RETROREFL), WHITE SYMBOL ON BLUE (RETROREFL) BACKGROUND - WHITE (RETROREFL)

*Ramp slope 5%
**See page 6-32
***See page 6-13 for area detail.

NOTE:
PROVIDE (1) 12"x18" SIGN @ EACH HANDICAP PARKING SPACE. EACH SIGN WILL BE ALUMINUM (PAINTED BLUE) WITH WHITE LETTERS AND THE INTERNATIONAL WHEELCHAIR SYMBOL. THE LOWER EDGE OF THE SIGN(S) SHALL BE AT LEAST 4" ABOVE GRADE, BUT NO HIGHER THAN 7" ABOVE GRADE. OWNERS PROJECT MANAGER SHALL APPROVE THE STEEL POST PRIOR TO INSTALLATION.

HANDICAP SIGN

NOT TO SCALE

REV.	DESCRIPTION	DATE

Design Professional's electronic or digital seal or signature is effective only as to that version of this document as originally published by Design Professional. Design Professional is not responsible for any subsequent modification, corruption, or unauthorized use of such document. To verify the validity or applicability of the seal or signature, contact Design Professional.



Date	FEBRUARY 2023
Scale	AS NOTED
Drawn	REK
Checked	STP
Project No.	3108-2201
Sheet No.	11

**Project: LEESBURG EXECUTIVE AIRPORT
NORTH APRON PAVING**



Town of Leesburg,
Virginia

Date & Time: Tuesday, March 21, 2023, 10:00 a.m.

	Name	Organization	Phone	Email
1	MATTHEW MADDEN	Atlantic Contracting	608-208-0486	Maddemm@acmpave.com
2	Michael Monr	MONA	301 440 1145	mmonr.moncontracting@gmail.com
3	Scott Coffman	TOL	703 737-7125	scoffman@leesburgva.gov
4	Michael Shields	TOL	703 737 7126	mshields@leesburgva.gov
5	Billy Myers	CHAMUNG CONTRACTING	540-829-7203	bmyers@daholding.com
6	JULIO VALENCIA	FINLEY ASPHALT	703-368-2287	FINLEY ASPHALT JULIO@FINLEYASPHALT.COM
7	SEB BOSTON	SLURRY PAVES	804-305-7575	SEB.BOSTON@SLURRYPAVES.COM
8				
9				
10				
11				
12				
13				
14				
15				